



As of 1/4/22

## 2022 COMPETITION GUIDE & RULEBOOK

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\* SCHEDULE UPDATES \* BREAKING NEWS**

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# **LA CROSSE FAIRGROUNDS SPEEDWAY 2022 GENERAL RULES & SPECIFICATIONS**

**PAGES 2-8 APPLY TO ALL DIVISIONS COMPETING AT LA CROSSE SPEEDWAY**

## **RULE BOOK DISCLAIMER**

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. NO EXPRESS OR IMPLIED WARRANTY OR SAFETY SHALL RESULT FROM PUBLICATIONS OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official.

The race director shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS. Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

## **PROMOTIONAL RELEASE**

Any competitor who wishes to compete, consents to the use of his name, pictures of himself and his car for publicity, advertising and endorsements both before and after the events, and relinquishes any rights to photos taken in connection with events, and consents to the publication or sale of such photos as the Speedway so desires.

## **THIS RULE BOOK IS A SUPPLEMENT TO THE NASCAR ALL-AMERICAN SERIES RULE BOOK.**

The rules and regulations in this book *have precedence* over the NASCAR All-American Series Rule Book, as they pertain to the weekly racing program at La Crosse Fairgrounds Speedway.

## **A WALK THROUGH A NIGHT AT THE RACES**

1. It is best to register your car the week before you intend to compete, although you may register the day you plan to race. Registrations are required each season. Go to the Pit Registration Office (orange building at the entrance of the pits). Tell the person on hand that you are ready to begin racing. Fill out the forms she or he gives you. (Remember-minors will need a special form) and he or she will assist you in selecting a number for the car (it's best to call the office for car number availability in advance).
2. On the night you are ready to race, come to the pit entrance building and register for the night if you have not already registered. Prior to every program it is necessary to secure a "Pit Pass" for pit entry. You must read and sign the "waiver and release". You may take a copy of the waiver and release form with you to read at any time. Proceed to the area of the pits and if you have not pre-arranged a designated parking space, please contact one of the pit officials. They should be able to show and tell you where to park, or find the correct person who can help you. Pit parking assignments take place at the orientation meeting (Late March / Early April), and after that, they are filled on first-come, first served basis.
3. Locate one of the La Crosse Fairgrounds Speedway Technical Staff to inspect your car, or he will tell you when and where inspections are to be held. Your car must be approved prior to competing in any events (including Time Trials).
4. Locate the race line-up board. It is on the back of the Pit Concession building. It is from this board that the line-ups and order of races are posted.

5. Now...It's time to Hot Lap. Provided you have made arrangements with the Tech Staff, get in your car, get ready to race - belts, helmet, shoes, and uniform in place - proceed to the track entrance. In most cases, a staff member will be here to check you and tell you when to proceed to the track. Hot Lap Sessions rotate by division, and are announced over the pit public address. It is currently the procedure to pull to the inside groove of the track and take several slow laps familiarizing yourself with the track. When you feel ready, and green light and flag are shown, speed-up and move into the upper grooves. **\*\*\*NOTE: AT ALL TIMES WHILE ON THE TRACK - BE AWARE OF WHAT IS GOING ON AROUND YOU - WATCH THE FLAGMAN, THE CONTROL SIGNALS, AND THE TRAFFIC.** When your session is complete, the flagman will wave the checkered flag. Enter the pits off the backstretch at ALL Times - Do NOT Use the Turn 4 pit entrance (Only Exception: during Qualifying).

6. After hot laps, return to your pit and wait for your division to be called for Time Trials (For Some Race meets Time Trials are eliminated). It is your responsibility to go when your division is called - **YOU WILL NOT BE CALLED INDIVIDUALLY.** Know when it is your turn to qualify, as a different "car number" is drawn to start each week's session. Cars qualify in order by car number - for those divisions that qualify (Late Models, Sportsmen).

7. Time Trials - Proceed to the back stretch of quarter mile (or designated staging area). When the official gives you the signal, pull out on the track and proceed around ready to get the green as you cross the Start Line. Make one complete lap (You are on the clock) and watch the flagman as you end your first lap - If checkered is displayed, proceed back to the pits. Watch the Flagman. Have your scanner on and listen to the officials. The top five are held in the turn four staging area until the completion of qualifying.

8. The races are lined up according to time or points average so now watch the lineup board for your number in a race for your division. While the race before yours is beginning, you should be in your car and lining up in the Staging Area (located behind turns 3 & 4 of the small track), in order shown on the board. When examining the board look at who is ahead of you, next to you, and behind you so you know where to line up at. Proceed on to the track when the staging staff gives the signal and realign yourself as you pull up to the start line. A note: We will never hold or delay a race while you are getting ready; get staged in plenty of time and have all your gear in place before entering the track surface. If for some reason you would like to or need to start at the back of your race, please tell an official.

9. **THE BIG MOMENT...**One thing we can't tell you is how to race so do your best but remember the following items. Watch the flagman...Know what the flags mean...if your division requires a scanner, be sure it is on and working....be alert and be aware...and **GOOD LUCK!** Have Respect for the track, your competitors' cars, and others' property.

10. Payoff...When the races are over, if you have won any money, it can be collected at the designated pay-off window (typically the beer stand in the grandstands). The pit stewards will announce when payout is ready.

11. Thank you for choosing to race at La Crosse Fairgrounds Speedway. Read the rulebook, know and respect all the rules, have fun, and if you don't know...PLEASE ASK!

## LA CROSSE FAIRGROUNDS SPEEDWAY GENERAL RULES

This general rule section applies to each and every racer, mechanic and/or pit personnel. Some *items*, obviously, do not apply to each class - you can easily determine which rules do not apply to you. You are expected to know the rules; ignorance will not be tolerated as an excuse.

### GENERAL RULE STATEMENT

“THE RULES AND/OR REGULATIONS SET FORTH HEREIN ARE DESIGNED TO PROVIDE FOR THE ORDERLY CONDUCT OF RACING EVENTS, AND TO ESTABLISH MINIMUM ACCEPTABLE REQUIREMENTS FOR SUCH EVENTS. THESE RULES SHALL GOVERN THE CONDITION OF ALL LA CROSSE FAIRGROUNDS SPEEDWAY EVENTS, AND, BY PARTICIPATING IN THESE EVENTS, LA CROSSE FAIRGROUNDS SPEEDWAY RACERS, MECHANICS AND/OR PIT PERSONNEL ARE DEEMED TO HAVE COMPLIED WITH THESE RULES. NO EXPRESS OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATIONS OF, OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS. THEY ARE INTENDED AS A GUIDE FOR THE CONDUCT OF THE SPORT AND ARE IN NO WAY A GUARANTEE AGAINST INJURY OR DEATH TO PARTICIPANTS, SPECTATORS OR OTHERS.”

### CONDUCT

1. Any fighting or reckless driving in the Pit area may subject the offender to suspension depending upon the seriousness of the incident and the decision of the Management. Any continuing problems from the same individual may result in permanent suspension. A fine of no less than \$100.00 may be assessed to the participants of any fight. NASCAR Rule Book may dictate fines and/or penalties for this offense.
2. La Crosse Fairgrounds Speedway is a member of P.O.W.A.R. (Promoters Of Wisconsin Auto Racing). Any member track of P.O.W.A.R that suspends a race team/ driver, that suspension may be upheld as well at La Crosse Fairgrounds Speedway.
3. We demand courteous conduct from all participants at all times. We do not tolerate profanity or obscene gestures in front of race fans, Officials, or Management. Profane signs on your car, sex signs, etc. are taboo; Swastikas or Nazi symbols on your car or clothes. We expect you to look like a professional and act like one! Be clean, uniformed and look respectable. **ANY ABUSE OF AN OFFICIAL WILL NOT BE TOLERATED.**
4. Penalties and fines for violation of the conduct, race, procedure, and general rules may be implemented per the NASCAR Rule Book to all participants (NASCAR members and non-members) at the discretion of the officials.
5. The possession, consumption or distribution of ANY alcoholic beverage or recreational/controlled substances at any time while in the restricted areas is strictly prohibited.
6. Officials will use the NASCAR procedure on ALL participants as it relates to alleged use, distribution or possession of drugs, narcotics or alcohol. By entering the restricted area, all participants agree to accept this policy and procedure Penalties are determined by the NASCAR officials.
7. At any time the display of any type of weapon or threat of bodily harm, may result in permanent suspension for the entire racing crew and may result in arrest of all parties involved.
8. When in the Pit Area, maintain a constant watch for your own protection. Drivers: Speeding through the Pit area is dangerous to everyone, and will not be tolerated!
9. For any race team, including but not limited to: driver, crew member, spectator, or relative that is associated with a race team - that suggests, makes threats thereof, and/or participates in any litigation against the La Crosse Fairgrounds Speedway or its operating company, will forfeit the privilege of participating or attending any events at the La Crosse Fairgrounds Speedway for a time specified by the speedway management.
10. Actions detrimental to racing. At any time, a Driver, Crew Member, Team Mate or ANYONE associated with that team, whose actions are determined to be detrimental to the Speedway, the Driver of said team will be suspended indefinitely. This includes all incidents at the Speedway itself as well of any public (Social) media platforms as well.

### LIABILITY INSURANCE

Our liability insurance insures the race track, the car owner and Sponsors. (Certificates are available for review by sponsors. Policy reviews and/or additional insured's can be arranged at prevailing cost.)

### **BENEFITS AND INSURANCES**

1. Participants legally entering the pits and/or restricted areas are covered by insurance for specific limits. When involved in an accident involving an injury, advise the racing Officials in charge so the necessary insurance forms can be handled properly. **NO CLAIMS WILL BE CONSIDERED UNLESS REPORTED PRIOR TO LEAVING THE SPEEDWAY PROPERTY THAT DAY / NIGHT / EVENT.**
2. For All participants: Any insurance claim filed on any injury sustained on Speedway property must be filed through your insurance carrier first. NASCAR's (or others) is supplement insurance and will cover any charges not covered by your insurance carrier. If you do not have insurance, NASCAR (or others) will become your primary insurance carrier.
3. Posted on the Pit Office wall is a certificate of benefits which covers everyone signed into the Pits. Call us or discuss any aspect of the insurance at any time. You and your crew members are covered with as broad benefits as the racers in the major racing associations in the U.S. If any question or problem comes up about benefits, contact the Speedway Management.
4. When involved in an accident involving an injury to you, advise the racing officials immediately so the necessary reporting will be accomplished. Contact the safety crew and give them a full report before you leave the Speedway. If physically unable, your crew must be responsible for this report.
5. Ambulance transportation fees are NOT covered by Speedway insurance.

### **GENERAL INFORMATION**

Every driver must inspect the racing surface and the race track to learn of any defects, obstructions, or anything which, in his opinion, is unsafe and they shall report that condition in writing to the Speedway Management. Any driver entering any racing event is considered to have inspected the track and all conditions are satisfactory to him/her...if not, **DO NOT RACE.** The participant further indicates that they are aware that auto racing involves risks and assumes the risks with full awareness and knowledge.

1. The Speedway assumes no responsibility for damage to or loss of your equipment, vehicle, tow vehicle, or any parts by any means whatsoever.
2. All cars are subject to a minimum specification inspection at any time. It is the responsibility of the driver to prepare their car FREE of defects and in safe racing condition
3. Passing "tech" does not imply your car is free of defects. You are personally responsible to be sure that your car is safe and race ready.
4. MINORS: See us well in advance so we may prepare the necessary minors' releases for your parent's' or guardian's signature. Don't wait till race day!! (10 yrs. thru 17 yrs. for pit entry)
5. Benefit and insurance questions must be presented in a timely manner.
6. We recommend you familiarize yourself with available benefits and insist that you understand the waiver and releases.
7. No one age of 9 or under are allowed in the pits or track at any time **without approval.** Parent and guardian must sign for all persons under 18 years old. The signed parental waiver and release must be completed and on file with the Speedway for anyone entering the pit area or restricted area whom is under 18 years old. NASCAR requires an additional waiver (Addendum "A") for anyone under 21. Touring series' have the discretion of pit age for their specific event. **"Pit Pay" is accepted at the Pit Gate and is suggested for use with minors.**
8. At any time, before, during, or after an event, officials may require additional measures of equipment or make additional determinations, as they deem necessary to further reduce any possible risk to competitors

### **PERSONAL SAFETY EQUIPMENT**

1. Each Competitor is solely responsible for the effectiveness of personal safety equipment used during an Event. NASCAR, TRACK OFFICIALS, OR THE PROMOTER ARE NOT RESPONSIBLE FOR THE EFFECTIVENESS OF ANY PERSONAL SAFETY EQUIPMENT.
2. Each Competitor is expected to investigate and educate himself/herself fully with respect to the availability and effectiveness of personal safety equipment available.
3. All entrants are recommended to have in their garage or pit area, if any, as part of their equipment, at all times, a fully charged minimum 10-pound dry chemical or its equivalent fire extinguisher with a visible, operating pressure gauge.
4. Helmets: Drivers are required to wear at all times on the race track a helmet, with a performance rating no less than SA 1995 or SA 2000 Standard Snell Sticker.
5. Drivers are required to wear eye protection for all divisions.
6. Drivers in all divisions are required to wear flame retardant gloves at all times.
7. Seat belts must be used and engaged for all events, and at any time vehicle is on racing surface.
  - A. A 3" shoulder harness and additional V-Type seat belt with a quick release is mandatory for any weekly competitor. Belts must be secured to roll bar cage and frame with aircraft quality bolts not less than 3/8 of an inch in diameter. A steel plate may be welded to the roll bar cage on the right side of the driver so the belt can be brought down in such a manner

- that it will prevent the driver from sliding from side to side under the belt. A shoulder harness inertia reel may not be used.
8. No person will be permitted to ride on the outside of any car or ON any trailer or hauler at any time.
  9. Nomex driving suits required for any division competing weekly. Nomex shoes are required for all Late Model drivers. At minimum, all other drivers shall use leather or other flame-retardant shoes. Nomex driving suits are recommended for all other divisions. At minimum, flame retardant clothing required for all other drivers.
  10. Aluminum racing seats required for any weekly division competing on the 5/8<sup>th</sup> mile track. No plastic or carbon fiber seat allowed. Seat must be fasted to frame. Additional right-side head restraints allowed.
  11. A. Window nets are required and used at all times for any weekly division competing on the 5/8<sup>th</sup> mile. A NASCAR type seat belt buckle method of mounting must be used and must be full size ribbon type.

#### **PRACTICE & TRACK RENTAL**

FOR ALL PRACTICE AND TRACK RENTALS, EVERYONE ENTERING PIT AREA MUST SIGN WAIVER AND RELEASE OF LIABILITY FORM. Anyone who is subleasing the track for testing, practicing, and/or any other activity, is the party whom is responsible for everyone entering the restricted area to have signed the waiver and release. The waiver and release must be signed by everyone inside the restricted area prior to any car entering the racing surface or restricted areas. Failure to do so may subject your race team to monetary fine, and/or loss of future sublease (track rental) opportunities. IN ADDITION, everyone inside pit area during track rental must sign Safety / Ambulance acknowledgement waiver. There are no Safety or Ambulance personnel on grounds for any practice or track rentals. Ambulance can be arranged with advanced notice of 5 (five) days for testing purposes at additional cost.

#### 1. THURSDAY PRACTICE

- A. Held every first and third Thursday of each month, starting the third Thursday in April, and ending after the third Thursday in September. Open practice sessions start at 5:00 p.m. and run until 7:00 p.m. Cars are to line up in the staging area to await their rotation. Each car is allowed a maximum of 8 laps per rotation. Cars may re-enter the line as many times as time permits.
- B. Other scheduling may preempt practice days.

#### 2. TRACK RENTALS

#### **NOTICE**

***Before entering the pit area, unloading a car or any other activity, all drivers and participants must complete rental forms and waiver / release forms at track office. Failure to do so will end session, forfeit all money paid and be subject to a cash fine.***

- A. The track is available for exclusive use at other times. Call track office for availability and reservation.
- B. Rental times are Half Day: 10 a.m. until 1:15 p.m. or 1:15 p.m. until 4:30 p.m., or full day 10 a.m. until 4:30 p.m., M-F.
- C. *There are no evening or Sunday rental periods.* Saturdays available only on scheduled race days, per availability. YOU MUST CALL TO RESERVE TRACK TIME ON SATURDAYS (or any other day) and to check availability.
- D. Rental fee is PER CAR. Contact Speedway for a rental fee pricing. Discounts may apply for multiple teams on the same rental date. Teams that do not compete weekly are assessed a 30% Ag Society Fee, payable to the La Crosse Co. Agricultural Society. Rental fees must be paid prior to departure of race track.
- E. A \$25.00 "clean-up" may be charged if area is not returned in like manner.
- F. Multiple cars may be allowed during a track rental session. **ONLY one car on the track at a time, at ALL TIMES.**
- G. The PIT GATE must remain CLOSED during all Track Rental Time.

#### **GENERAL POLICY**

1. ANY DISAGREEMENT OVER TECHNICAL QUESTIONS OR OPERATIONS WILL BE RESOLVED BY THE RACING OFFICIALS AND MANAGEMENT. WHEN THEIR DECISION IS RENDERED, SUCH DECISION IS FINAL AND BINDING.
2. The Speedway Management and Officials will establish the length, frequency and administration of all events and programs and when their decisions are rendered, that is FINAL AND BINDING. Exceptions to the rules and specifications may be made on a temporary basis at the discretion of the Speedway Management.
3. The Management and Officials will determine all finishing positions and their decision is final!
4. Any complaints, disputes, questions or problems must be directed to the Speedway Management immediately following the event for their action or follow NASCAR Rule Book.
5. It is the duty of all drivers, car owners and mechanics to bring to the attention of the Officials any unsafe equipment or practices, or any rule infraction of any car or driver.
6. Continuous developments in racing may necessitate changes that we cannot anticipate at the time rules are formulated. Hence, we may, if necessary, update, modify and add to, or delete rules. The "Except in Rare Instances" (E.I.R.I.) rule is always applicable. E.I.R.I. is in effect at all times.
7. Shoes, shirts and pants are required on all personnel at all times in the pit area.

8. You must be 16 years old to participate as a driver in any weekly or organized division competing on the "big track" at this speedway, 15-year-old drivers are allowed to compete with speedway approval. Touring series drivers' restrictions are subject to individual series' rules. You may compete at the age of 14 in the High School Racing Division (1/4 mile).
9. Every racer is expected to participate in every event in which is possible for him/her to run. When a racer refuses to participate because of his disagreement with the length, type, or style of the race he is subject to suspension for a period determined by the Management.
10. Any car involved in a serious accident, may be impounded by the speedway or by the sanctioning body.
11. ALL CARS MUST be identified with a number of at least 20 inches or more in height and 18 inches in width must appear on both doors of the car in contrasting color. Cars must display roof top numbers of 24 inches high readable from the right side. No metallic, reflective, or foil numbers are allowed. Be sure your car looks great from a distance as well as up close so fans and scorers can easily identify your car on the track.
12. For all divisions, two tow-cables or hooks must be located on front and rear of cars. Cars without proper hooks will be removed as Safety and Wrecker crews see fit.
13. For weekly divisions- driver's last name and number must be placed on top of windshield at least 5" tall. Driver's Twitter "handle" or likewise may be used instead of name.
14. This rule book does not address all aspects of a race car in detail. Any variance or interpretation to the rules is the Tech Official's discretion - not yours, thus any part, altered part, item, equipment not specified or approved by Tech Officials, will be declared illegal.

### **RACE TRACK OPERATING PROCEDURES**

1. The only people allowed on the racetrack and outside the infield pit guard rail boundaries are Racing Officials and Speedway Management. Everyone must stay away at all times unless requested by official to assist for some special reason. Drivers may seek a place of safety in the infield following disablement...when crashed, stay in your car. DO NOT get out and inspect the damages on the track. NEVER EXAMINE YOUR CAR WHILE THE CARS ARE RACING ON THE TRACK!! Pit crews, car owners, and personnel related to any car are not allowed on the track following an accident or injury.
2. Do not enter the racing surface without express authorization. Official will O.K. your entry at the track entrance(s) and signal for you to proceed (i.e., cross-over to pits and exit).
3. The track may not be used for practice at any time other than designated in these rules.
4. Any competing vehicle whose speed has been reduced to a point where it causes a safety problem or slows track activity may be removed from the racing surface at the discretion of the Officials. Watch for black flag or listen to scanner.
5. Our racing program is based on the availability of enough vehicles qualified. If this number is not available, alternate scheduling or programming will be used. Racing events will be "lined up" in various ways to accomplish the best racing program.
6. A driver may change cars (with approval of tech officials) prior to start of race. (Start of race is the field accepting the "one to go" (until green flag) signal by the flagman). Driver must start at rear of field. Any variations of this rule must be approved by the Director of Competition and/or Management.
7. A driver may re-qualify an additional car. However, doing so disqualifies the first car and the qualifying time.
8. No driver may get out of his car on the track or infield to argue or discuss the race with the Starter or Officials. If this rule is violated, the driver may be disqualified for the event or suspended according to the decision of the Officials.
9. Any event is completed when over one-half of the scheduled length (laps OR time) has been run by the leader or terminated by the Officials for safety reasons.
10. Any participant who refuses to allow Tech Officials to inspect their car will be subject to the following: First refusal - loss of points and money for night. Second refusal - loss of points for year, loss of money for night, one race event suspension, third refusal - loss of money and all points earned, suspension for remainder of year, and a \$100 fine (ineligible to compete until paid).
11. NASCAR confiscation rule in effect for ALL divisions. Any team refusing to relinquish a confiscated part will be immediately suspended from all speedway operations for a period set forth by the speedway.
12. A division may be lined up from qualifying. Any team that turns two or more (consecutive) laps during competition that are "faster" than their qualifying time is subject to a one lap penalty. Any lap greater than .350 for the Late Models and Sportsmen is considered "faster".
13. No hauler over 9'6" in height is allowed in pits during racing events.
14. At no time is any team member or driver allowed to stand ON or BEHIND the 1/4-mile wall. This blocks the view of the spectators.
15. A race team will forfeit two times the number of earned points and fined two times the amount of purse won per violation - for any race team, whose registered driver has been replaced before or during a race event, by any other driver, without informing the proper officials of the driver change. Fines must be paid before car and drivers may compete again.
16. A car left on Speedway/Fairgrounds property without approval for more than 48 hours will be considered abandoned and be removed at owner's cost.

17. Race teams are required to help dry racetrack in inclement weather. Failure to do so may result in loss of starting positions or race team may not be permitted to participate in the race event itself.
18. Absolutely No "for sale" signs allowed on cars while on the racetrack.
19. **This Speedway uses a "fail to tail" system on yellow flags, any cars involved (a spin to avoid, or stopping is considered involved) will go to the rear of the field.**
20. Compression limits and testing: For 9:1 motor -compression ratio of 9.0:1 is set; a tolerance of .4 is allowed, 9.5:1 will be declared illegal. For 10.5:1 motor -compression ratio of 10.5:1 is set; a tolerance of .3 is allowed, 10.9:1 will be declared illegal.  
Compression ratio is defined as the difference between actual cylinder volume at B.D.C. and T.D.C. which will be determined by the volume gauge and/or electronic sonic tester (whistler). The whistler reading is final. A "re-check" can be requested by race team, however, the re-check must take place within 20 minutes of original test. An illegal engine will result in the following penalties:
  - 1) Loss of earned points and monies won from that day's event.
  - 2) A cash fine of \$400.00 in lieu of confiscation may be assessed.
  - 3) Off premise compression testing is \$15.
21. **Transponders required for each team competing on the 5/8<sup>th</sup> mile and for Hobby Stocks and Hornets.**  
Transponders will be available for rent each race night. Race finishes are determined by the race tower exclusively. Transponder must be installed no more than 12" above track surface, with unobstructed path to track, and arrow pointing down. **Location is mandatory: forward from center of rear axle to center of transponder is 25 1/2"**.
22. Race Teams are subject to a starting position penalty if they fail to be staged in lineup, by proper time for their race. "Proper time" is typically when white flag is displayed for current race.
23. A "break out" time may be implemented in the Sportsmen division – See Sportsmen Division rules for information.

#### **Miscellaneous**

1. Drivers (only) in the Late Models, Sportsmen, Hobby Stocks, and Hornet divisions are required to have an operating scanner or Raceceiver. Any driver without an operating scanner will NOT be allowed to race. Any driver not heeding the tower's commands through the scanner will be sent to the rear of the field or pits. The scanner/Raceceiver frequency is: **454.000**. No two way or one-way communication is allowed from driver to crew (or any other party). The only communication allowed is through the Speedway's race control and the driver via the scanners.
2. There is a 30 minute "no move" rule in effect at the end of the last race. This means no race team or haulers may enter or leave the Speedway until approved by Tech Official. Teams that have special circumstances may leave early, but **MUST** contact a NASCAR Official **first**.
3. DECALS- Race teams are required to display the proper decals in the proper location to receive points and pay.

#### **PAY-OFF PROCEDURES**

1. All points shall be earned by the starting driver. All payments will be made to the starting driver, unless prior arrangements have been made in writing.
2. Payoff will be made only for events actually COMPLETED. Never will any event be paid when it was not run.
3. When a program is shortened by rain or some unforeseeable or fortuitous event, yet is considered complete and no rain checks issued, payoff will be made for events completed. In this event, a lengthened feature may be run at the next practical time.
4. All race results will be announced before payoff. All winnings will be paid at the designated pay-off window (Typically the Beer Stand). **DRIVER MUST BE A NASCAR AND LA CROSSE MEMBER TO EARN POINTS AND CLAIM PURSE WINNINGS.**
5. A driver has a maximum of two weeks following event held to pick-up payoff. Money not picked up will be forfeited.
6. Any driver, car owner and business receiving and pay out must have a W-9 on file with the Speedway. No W-9, no pay.

#### **TEAM RESPONSIBILITIES**

1. At any time the conduct of any team member or driver is a discredit to the Speedway, the racing industry, or to himself, he or she may be removed from all racing activity at the Speedway. **The driver is responsible for all the people associated with his car and/or signed in with the car (this includes the grandstands and any social media platform).**
2. Responsibilities of the driver and crew are addressed in the NASCAR Rule Book and will prevail in most cases.

**TRACK POLICY**

The La Crosse Fairgrounds Speedway and the La Crosse Interstate Fairgrounds are private property. Any person on this property without the permission of La Crosse Interstate Fair Association and Motorsports Management Services, Inc. is guilty of trespass and subject to the penalties prescribed by law. Through your registration or association, you have been given the authority and the right to be on this property in conjunction with racing activities. However, the Administration of La Crosse Fairgrounds Speedway reserves the right to revoke and cancel this authority at any time that it is felt that your presence or conduct is not in the best interests of the sport of auto racing, your fellow competitors, the fans, the management and the employees of the speedway.

**MANAGEMENT CLAIM RULE**

**HOBBY STOCKS / HORNETS / STREET STOCKS / 6 SHOOTERS:**

1. Management has the right to claim any car at any time. The Claim amount is \$950 for Hobby Stocks, \$650 for a Hornet, \$500 for a Street Stock or 6 Shooter. Drivers may keep seat belts and racing seat.
2. If you refuse a legal claim, you will forfeit any points, trophies and prize money for the entire season. Car and driver will be ineligible to compete for the remainder of the season.

**POINT AND TRACK CHAMPIONS**

**SPORTSMEN**

The 2022 Point Champion will be allowed to compete in Sportsmen Division the following year of their Championship.

**HOBBY STOCKS**

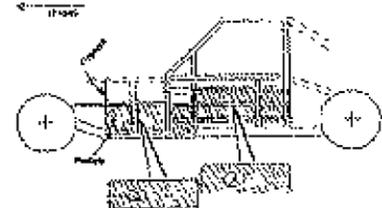
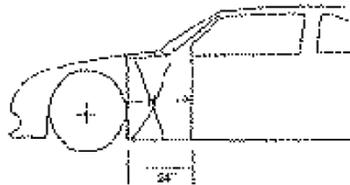
The 2022 Point Champion will be allowed to compete in the Hobby Stock Division the following year of their Championship

**HORNETS / STREET STOCKS / 6 SHOOTERS**

The point champion is ineligible to compete in his/her respective division the following year.

**REQUIRED PATCH AND DECAL LOCATION FOR ALL NASCAR DIVISION CARS**

1. All cars must provide the area shown below for the current series sponsor decals. The exact placement will be distributed with the decals. In addition, provided "division" sponsor's decals are required in the location designated by the speedway.
2. The NASCAR Contingency decal area MUST NOT contain any other lettering, decals, or decorations.
3. NASCAR Series patch MUST be on left side of driver's uniform, La Crosse Fairgrounds Speedway patch on the right side.
4. Division sponsor decal must be visible on race car at all times to compete, receive payout, and points. This includes car shows, parades, etc...



**DOORBAR DEFLECTOR PLATE**

TYPICAL DRIVER'S DOORBAR NETWORK

All Divisions on 5/8 Mile Track  
(1 3/4 inch, .095-wall tubing)

DEFLECTOR PLATES

Approximate Dimensions:

Plate A - 10" x 30", 1/8" Thick    Plate B - 10" x 30", 1/8" Thick

NOTE: 11 gauge is .119 thick and weighs 5-lb./sq. ft.

600 sq. in. = 4.17 sq. ft.

Approximate weight - 20.8 lbs.

***END OF GENERAL RULES SECTION***

# NASCAR LATE MODELS

*This Division will compete under the sanctions of NASCAR. The specifications listed will become part of the official NASCAR Rule Book, except in cases of obvious conflict. All technical rules will generally fall within NASCAR and La Crosse Fairgrounds Speedway specifications. Any disputes will be resolved by NASCAR officials. All drivers in this division must be a member of NASCAR to compete.*

## BODY AND APPEARANCE

1. Tech approved American Made production model of 2010 or newer.
2. Convertibles, station wagons, truck bodies allowed with approval of the Speedway Management. ALL BODIES MUST BE STOCK APPEARING.
3. All cars must have complete bodies, fenders, and trunks in proper configuration for the model and year. No car may qualify or race minus any part (without tech approval).
4. Body must be centered within 2 inches, based on distance between outer edges of tires.
5. Bars, plates, or pipes may not be welded or attached to the outside of any part of the body except a 1 inch by 1 inch rub rail touching the body during its entire length from wheel well to wheel well. It must be painted the same as its background.
6. Body must conform to the ABC bodies rules. All body panels shall remain as produced per the ABC bodies guidelines and may not have any additional holes, vents, ducts, bubbles, etc.
7. No aerodynamic devices such as fixed or moveable skirts or barriers to modify airflow beneath, below or around the car permitted. No wings, belly pans, or vertical/horizontal air stabilizers.
8. Nosepiece of all body styles must be installed at the produced stock angles. Front nosepiece must be as produced - with no modifications or add-ons. (approved repairs allowed).
9. Rear end of the car must be completely sealed, including area for tail lights. Rear bumper cover must be stock or molded rubber or fiberglass.
10. Bodies may be steel, aluminum or fiberglass, provided stock appearance is maintained. No Kevlar parts allowed.
11. Rear spoiler must be mounted in normal position. Must be single plane only. This means no excessive forward verticals, gussets, sideboards and no wrap-arounds. Spoiler may not exceed five (5) inches at highest point. Maximum width of rear spoiler is sixty (60) inches. Spoiler must be made of a clear "see through" material.
12. Bodies must conform to templates and/or measurements supplied by speedway. The ABC body is allowed and must pass the "referee tech template".

## WINDOWS/OPENINGS AND WINDSHIELDS

1. Must run a LEXAN windshield and clear (no tint) Lexan or Plexiglas rear window. Minimum thickness of 1/8".
2. Length, size and area of windshield and all windows must be stock size angle and configuration.
3. Approved side-vent fill-ins allowed, and may not exceed seven inches at lowest point and continue 90 degrees upwards from that spot.
4. Window nets required and used. Window nets must release from top. Window nets must be attached to roll bars only. Ribbon-style Window Nets only. NASCAR type seat belt latch to fasten window net required.
5. A minimum of two front windshield and rear braces must be provided to maintain stock configuration at any time. Recommended braces are installed to chassis.
6. Windshield pillars must be in stock position and have stock appearance.
7. Opera window vents allowed.
8. Brake ducts allowed with approval of Tech Official.

## CHASSIS/FRAME

1. The front stub frame must be a production model made by Ford, GM, AMC or Chrysler forward of the firewall, and at least 24 inches rearward of the spindle center. Frame must be finished out rearward.
2. The fabricated front stub frame must follow production Ford, GM, AMC or Chrysler layout (single frame rail, front cross-member design), front upper spring mounts must be directly attached to frame rail. Fabricated stub must be made a minimum of 2" wide, 3" tall. 120" wall rectangular steel tubing, all joints must be gusseted.
3. Ford struts may run rearward. Struts may dissect frame for travel only.
4. **There is NO minimum ground clearance rule, chassis will be placed on 4" blocks to confirm correct height of body components and additional measurement**

**GROUND CLEARANCE**

1. There is no minimum frame and undercarriage ride heights.
2. All body parts 4 inches.
3. Front Spoiler 4 inches.
4. Everything behind rear wheels, except frame rail 10 inches (IROC Taper-on body allowed to the discretion of tech official)
5. Those clearances are absolute!!

**WEIGHT**

1. Base weight 2825 lbs.
2. Minimum weight must be met at any time with the driver in the driver's seat, hands on steering wheel, with helmet on head.
2. Right side BASE weight of all cars shall be at least FORTY-TWO (42) percent of the car's current weight.
3. All added weight must be in 5 lb. minimum units and painted white AND NUMBERED TO MATCH THE CAR.
4. Weight must be attached to the frame structure. Do not mount to firewall, fuel cell, battery mounts, floor pan or within driver's compartment. Weight attached behind the axle may not hang below frame rail (including kick-ups) and will be inspected for mounting.
5. Any loss of weight while racing will result in a cash fine of \$2.00 per pound.
6. Weight determined by the track scale only.
7. Minimum front-end weight is 49% of current total weight at any time.
8. Weight must be declared by race team and made visible to the Tech Official at scales.
9. Weight deductions:
  1. 50lbs for stock OEM cast exhaust manifolds (see Tech Man for approval)
  2. 25lbs for cast iron intake (on 9:1 motors).
  3. 25lbs for a 9:1 motor
  4. 25lbs for clutch 7.25" or larger.
  5. 25lbs for approved common shock.
  6. 25lbs for exhaust exiting rear of car with a downward deflector.
10. Weight additions:
  1. 25lbs for a 10.8 compression motor.
  2. 25lbs for Ford "N" heads.
  4. 50lbs for cambered rear ends.
  5. 100lbs (minimum) for LS Motors (subject to tech approval)
  6. 25lbs for ratchet type differential.
  7. 25lbs for declared track width greater than 65" (must be less than 66").
  8. (Winners Equalizing Percentage (WEP) - 20 lbs. added for each feature win. Driver will carry 10# of that weight from that feature win for the remaining regular season; the other 10# can be removed after the third night of competition following the feature win. Feature winners' weight applied at next race event they are competing in. After a third feature win, in addition to the extra weight, the right-side weight percentage will increase be ½% for that win, and every feature win thereafter for the regular season, with a maximum of 2%.
11. Each race night, teams are responsible for declaring what the car needs to weigh for that event. The posted weight will need to be readable and accessible near the driver's door (A-post). Weight posted would be the sum of base weight plus or minus any weight adjustments subject to that car. An inaccurate posted weight will subject the car to disqualification.

**WHEELBASE**

1. The shortest wheelbase acceptable is 104 inches measured from centerline of front wheel to centerline of rear wheel. A tolerance of 1/2" allowed.
2. Maximum track width 65 inches, at spindle height (front and rear) - 1/2" tolerance as measured with the referee.
3. A Wide Width can be declared by the driver of up to 66 inches but 25lbs will be added to the weight.

**TRANSMISSION/DRIVELINE**

1. American made passenger car O.E.M. manual transmissions: 3 speeds must have 3 operable forward gears; 4 speeds must have 4 operable forward gears. Body must have an operable reverse gear.
2. One or Two 360-degree driveshaft hoops required.
3. One-piece steel or aluminum driveshaft only, Minimum diameter 2&1/2 inch O.D.
4. Driveshaft must be painted white.

5. Transmission "quick change" units not allowed.
6. No automatic transmissions allowed.
7. Transmission claim: Feature finishing positions 1-5, may be claimed by feature finishing positions 6-10. Claim would consist of \$50 cash and a swap of transmissions. Only one claim per driver (car) per year. CLAIM MUST BE APPROVED BY SPEEDWAY. Refusal of claim would result in a disqualification.
8. Stock style shifter required.

### **REAR ENDS**

1. Any Conventional closed type rear end acceptable.
2. Solid Steel axles. No gun drilled allowed. **No exotic or torque release axles allowed.**
3. Quick Change rear ends acceptable.
4. A \$20.00 fine will be assessed to any car whose rear end plug is not secured while on the racing surface.
5. A gear rule may be implemented per technical bulletin.
6. Cambered rear ends allowed with a maximum of ½ degree.
7. Detroit locker (ratchet type) allowed (add 25lbs).

### **BRAKES**

1. Four-wheel brakes required at all times.
2. Brake systems must be conventional hydraulic type.
3. Single, dual, or four piston (steel or aluminum) calipers with maximum retail price of \$190
4. No thermal lock pistons allowed.
5. Brake pedals must be in stock location.
6. Rigid mounted rotors, maximum diameter of 12 ¼ inches, no drilling permitted, no ceramic coatings.
7. No floating calipers or rotors, no self-centering rotors.
8. No ABS units or brake recirculation systems.
9. Only one brake bias adjusting unit per car.
10. No electric wheel fans or blowers allowed.

### **CLUTCH**

1. Clutch disc with steel hub, completely steel pressure plate and steel, cast iron or aluminum flywheel are required and must be of stock type and size. A functional full circle fiber faced clutch disc with a minimum diameter of 7.25". No button clutches.
2. Only two, one-inch holes, placed front to rear with access from top of car, with clutch disc and pressure plate visible must be provided. One 1/2" hole must be placed directly above flywheel teeth at 12 o'clock, for aid in compression testing.
3. Steel scatter shield required.
4. Multiple disc, small clutch or other than stock allowed. However, it must:
  - A. Must be disc/pressure plate type clutch.
  - B. No cone or coupler type allowed
  - C. Clutch larger than 7.25" (deduct 25lbs). No clutch smaller than 5.5" allowed.

### **SUSPENSION**

1. Conventional mounted 5" springs or coil overs allowed. Springs must be magnetic steel and not to exceed \$125 retail price. Springs must maintain equal coil separation and not be tapered. Springs must be able to freely move and not have any type of binding.
2. Upper A-Frame may be tubular type but must be conventional "A" design and inner pivot points not exceed 12 inches, and must have ball joint and mount from the top. Mono balls allowed.
3. Stock lower control arms and must have a ball joint.
  - A. Modifications allowed, but must maintain stock components, design and appearance.
  - B. Both fabricated and stock stub cars may use fabricated lower A-frames/control arms.
  - C. No Moveable lower spring cups.
4. Only one shock per wheel. The maximum retail value of all components not to exceed (to all competitors) per shock is **\$325.00**.
  - A. Shock absorbers and components must be available to all Competitors from the shock absorber manufacturers.

- B. Track Officials may use a shock absorber and internal components provided by the respective manufacturer as a guide in determining whether a competitor's shock absorber and internal components conforms to the specification of the Rule Book.
- C. The only shock absorbers and internal components permitted will be those approved by Track Officials.
- D. Modifications or changes to the shock absorber and internal components will not be permitted.
- E. Shock absorbers and components must be used as supplied by the manufacturer.
- F. All approved internal components must be used in only their respective manufacturer's shock absorber.
- G. External adjustments will not be permitted on any shock absorbers.
- H. Shock absorber base valves will not be permitted.
- I. "Steel" deflective disc valve shims must seal the primary metering faces of the single piston in the main shock body.
- L. The shock must be mounted in a reasonable location so as to not act as a bump stop.  
Shocks must have travel and not be "solid" in any form.
- M. No bump stop/rubbers, compression/rebound-limiting or coil bind set ups.
- N. No chains, bolts, straps, etc. No shock covers.
- O. The shock body must be the same measurement as the shock shaft. Ex:) a 7-inch body must have a 7-inch shaft.
- P. Shock absorbers utilizing a gas reservoir:
  - 1) Inert-gas pressurized shock absorbers must be mono-tube, deflective disc valve type with an integral gas reservoir.
  - 2) Only a single one-piece piston is permitted in the main body with one (1) shim stack on the compression side, and one shim stack on the rebound side and only a single floating piston will be permitted in the integral gas reservoir.
  - 3) The gas reservoir, maximum outside diameter must not exceed 2.600 inches.
  - 4) The divider piston must be installed to the manufacturer's specifications without any modifications.
  - 5) External shock absorber gas reservoirs will not be permitted.
  - 6) The gas reservoir must not be filled with any material other than in an inert-gas form. Oils or approved by Track Officials will not be permitted in the gas reservoir side of the shock absorber divider piston.
  - 7) An external Schrader / needle valve to pressurize the shock absorber with gas will be permitted.

**NOTE: A common shock is the goal for a majority of tracks in Wisconsin/Midwest. The first step is offering a weight break for the approved common shock. Future rules will continue to add weight for the non-conforming shocks and a weight allowance for the approved common shock. The current approved common shock is: KONI: 30 SERIES Part Numbers 7325, 7436, 7499, 7647, 9325, 9436 (adjustable, non-rebuildable) mandatory 7" on the front and 7" or 9" on rear only, bump stop enclosed in KONI package will NOT be allowed. There is a 25lb weight deduction for this shock.**

- 5. Rear suspension may use leaf or coil springs.
- 6. No cantilever or rocker arm rear suspension.
- 7. Trailing arm tubes must be a one-piece solid tube. (No rubber or springs allowed). No floating type rear axle housing mounts.
- 8. Aluminum replacement components of entire suspension system and chassis are NOT allowed, except:
  - A. Tie rod tubes      B. Rack housing      C. Trailing arms      D. Rear rotor hats      E. Rear hubs
  - F. Quick change center sections and side bells.      G. Wheel adapters and spacers, drive plates.
  - H. Heim joints      I. Third link bracket      J. Third link tube      K. Lower strut tubes
- 9. No device, mechanical or electrical for shifting weight will be permitted inside driver's compartment. No hydraulic weight shifting devices allowed.
- 10. Approved third link only (must be one-piece). Must be solid (no springs).
- 11. No rear sway bars allowed.
- 12. The droop eliminator on the rear axle must be fixed mounted that is not adjustable and has a minimum of 1.5 inches clearance to the rear axle when the car sits on all four wheels.
- 13. No preloading allowed on coil-overs.

### **SPINDLES/HUBS**

- 1. Any steel spindles allowed. Must remain of stock design. Aluminum steering-arm and ball-joint mounts allowed.
- 2. Aftermarket hubs allowed with 5/8" wheel studs. (Maximum retail price of \$325) No gun drilled studs allowed.
- 3. Wide 5 hubs and spindles allowed. (Wheels must meet minimum wheel weight)
- 4. No oil filled hubs.

### **STEERING**

- 1. Any type of conventional steering system allowed.
- 2. No ratio multipliers or one-piece shafts allowed.

3. Quick-release required.
4. Steering wheel "Nose Pad" required. Collapsible steering shaft recommended.
5. Power steering is allowed.

### **WHEELS**

1. Steel wheels only. Maximum width 10". Maximum measurement inside bead to bead. May be reinforced (non-ferrous wheels made by riveting components or bolting components are not allowed).
2. "Superlight or ultra-light" wheels will not be allowed. Minimum wheel weights:
  - A. 17 lbs. wide-5
  - B. 21 lbs. for G.N. style (5x5).
3. Wheels will be weighed without tires and balancing wheel weights.
4. Bleeders not allowed. Lug nuts must be magnetic.

### **TIRES**

1. The 2022 tire will be the 10" Hoosier 1070. The Hoosier D800 would be allowed to compete but would be subject to Speedway approval and would have to start at the rear of all events.
2. Tires must be purchased through authorized speedway dealer. (See "Concept Tire Rules" in back of book)
3. Tires are subject to change adjustment and alteration.
4. No system of heating, soaking, sipping, grinding, buffing or changing the composition, coding or character of tire allowed. Note: anyone caught using or CARRYING tire softener in their hauler or pit area will be fined \$100.00 and immediately suspended for two weeks.
5. A tire "Minimum Softness" may be declared each race night. "Soft" tires will be confiscated.

### **ENGINE**

#### **GENERAL ENGINE RULES**

1. Any Domestic engine may be run in any body style of standard production.
2. The engine must be located so at least one spark plug hole is even with or ahead of a line, 2 inches back from the center of the spindles. For Ford engines that cannot clear the oil pan at the front cross member, a maximum of 3 inches will be allowed, but the crank height will be at least 12.5 inches.
3. Minimum crank height 11 inches. From centerline of crank. For Ford engines that cannot clear the oil pan at the front cross member, and have greater than a 2-inch setback but less than 3 inches, the crank height will be at least 12.5 inches. The engine must be located so at least one spark plug hole is even with or forward of a line projected between the upper ball joints with the caster, right and left, at zero degrees and the frame rail level. Dodge and Toyota motors: see tech for setback.
3. Minimum crank height 11 inches. From centerline of crank. (see above for exceptions)
4. The lateral (side to side) location of the engine, measured from the cylinder heads, must be centered between the centerline of tires within two inches. (The difference in the measurement from one side and the other may not exceed 4")
5. No external oil pumps or dry sump systems. Altered oil pans & oil coolers optional. No aluminum oil pans.
6. No roller cams, no hard face overlay cams or mushroom tappets. Maximum valve lift .600
7. No automatic cam timing devices, no belt drive.
8. Stock appearing type harmonic balancers only. (No minimum.)
9. Engines must be of a standard production corporate model not to exceed a maximum of 362 cubic inches. Maximum overbore of .060 and stock stroke only.
  - A. No 180° crankshafts.
  - B. No lightweight crankshafts.

#### **9:1 ENGINE & 10.5:1 ENGINE**

1. Cast iron block with a maximum of 362 CID.
2. Must use flat top style pistons.
3. Intake: Stock O.E.M., street application, single carburetor, cast iron intake (deduct 25 lbs). No modifications of any kind allowed (the Bow Tie intake would fall under this configuration), or Edelbrock Victor Jr. 2975 aluminum intake allowed. EGR may be plugged.
4. Cylinder Heads
  - A. Cylinder heads must be UNALTERED original "as cast" Form, with the following exceptions:
    - 1) Multiple angle valve grinds permitted. However, maximum angle not greater than 60 degrees may be done. Grind must be done with a steel cutter (no stones).
    - 2) Push rod holes may be enlarged. However, they may not exceed 1/8". No lightweight pushrods allowed.
    - 3) Combustion chambers may be "cc" matched by resealing the valves ONLY.
    - 4) Intake, exhaust and head gasket surfaces may be re-machined (milled) to improve sealing.

- 5) Head may be cut for valve seals.
- B. Exclusive of the above exceptions: any cylinder heads with evidence of sanding, polishing, relieving, grinding, porting, angle milling, chemical treating, or abrasive blasting to the original cast form will be declared illegal.
- C. With exception of valve guide material. NO additional material of any kind may be added to the cylinder heads.
- D. Valves must be of stock size for that head. Minimum valve stem size is 11/32. Valves must be solid, no hollow valve/stems allowed.
- E. Any stock style valve springs and retainers allowed.
- F. Screw in studs allowed.
- G. A maximum rocker arm ratio may be declared.
- H. No shaft mounted rocker arms. Some manufacturer exceptions apply.
- I. Stock or stock replacement cast iron unaltered heads with factory valve angles (no angle milling, porting, polishing, acid dipping allowed). GM Bowtie numbers 14011058, 10134392,(casting number 14011034 and 12480034), World Products Sportsman II numbers 011150, 011250 allowed.
12. Vortec heads are not allowed.
13. For 9:1 motor -compression ratio of 9.0:1 is set; a tolerance of .4 is allowed, 9.5:1 will be declared illegal.
14. For 10.5:1 motor- compression ratio of 10.5:1 is set: a tolerance of .3 is allowed, 10.9:1 will be declared illegal.
15. Compression ratio is defined as the difference between actual cylinder volume at B.D.C. and T.D.C. which will be determined by the volume gauge and/or electronic sonic tester (whistler). The whistler reading is final. A "re-check" can be requested by race team, however, the re-check must take place within 20 minutes of original test. An illegal engine will result in the following penalties:
- 1) Loss of earned points and monies won from that day's event.
  - 2) A cash fine of \$400.00 in lieu of confiscation will be assessed.
14. Stock appearing valve lifter only (no roller or mushroom type). Diameter of lifter and lifter bores must be stock as produced for that engine. Lifters must be stock type steel, no composites & no ceramics. Must be magnetic. Lifter bores may not be sleeved with exception of block repair - maximum 3 bores on any block.
15. Titanium retainers allowed. NO other titanium parts allowed.
16. Camshaft bearings must remain stock babbit type (No roller).
17. No plastic or aluminum blocks.
18. Connecting rods must be magnetic steel.
19. OEM style rocker arm mounting required.
20. No crankcase vacuum pumps.
21. Oil pan inspection plug mandatory.
22. Stock OEM firing order. Exception: #4 and #7 may be switched (only).
23. If you have any question on the legality of your heads, it is your responsibility to contact the late model tech official, before you put your engine together
24. Any engine may be impounded to check the internal components.
25. NOTE: Weight reductions for 9:1 engine = 25lbs. Weight addition for 10.8 engine = 25lbs.

### **CRATE MOTOR**

1. A crate motor must be completely and remain completely sealed by factory to be eligible to compete.
2. Motors must be purchased through the Speedway. Motors may be purchased from other local teams with the approval of the Speedway.
3. Exhaust manifolds (headers) must have primary pipes 1 5/8" or less into a 3" collector. Must use approved mufflers with headers.
4. Weight penalties, if any, will be reviewed at the technical staff's discretion.
5. The only motors allowed: GM 88959604 or 19318604. Maximum compression can never be greater than 9.8:1
6. The RPM chip is set at 6400 rpm.
7. At the Speedway's discretion, to ensure the integrity of the crate motors, the Speedway has the right to "swap" a (new) crate motor with a competitor after the driver or car has won 5 features during the regular season (Oktoberfest dos not count). Note: Any driver or car that has 5 or more of the fastest qualifying time of a race night- that will also be considered as a "feature win". Location and time for the swap to be set forth by the Speedway and swapped motor must remain in original unaltered condition as when the swap is announced.

### **LS MOTORS**

Because of the continuous evolution of the LS Motors, no parameters can be set at this time other than the "home track" rules apply. An LS Motor may compete, but not more than two times during the regular season and will be subject to a minimum weight penalty of 100lbs.

**CARBURETOR**

1. Acceptable for the 9:1 or 10:5 motors are the two-barrel, Holley 4412 500 CFM carburetor. Holley Ultra series carb. not allowed. The Holley Aluminum (Part #0-4412SA) 500cfm carburetor is not approved/allowed. No alterations of and/or no metal removed from the carburetor, except enlargement of idle hole in throttle plate. For the Crate Motor: Holley 650-HP (PN-80541-1) carburetor. No alterations of and/or no metal removed from the carburetor, except for enlargement of idle hole in throttle plate. The Holley Ultra XP carburetors are not allowed.  
A Spacer plate plus gasket (of no more than 1.5/8" total) allowed. Straight bore for Crate Motors. Tapered allowed for non-Crate Motors
2. Choke housing may not be removed. Choke "Butterfly" may be removed.
3. Adaptor plate and/or spacer and gasket may not exceed 1 1/2 inches.
4. Jets may be changed. No dial-a-jet devices.
5. Carburetor restrictor plates may be required on all cars. Plates will be supplied by NASCAR officials. Opening size will be determined by NASCAR officials. Plates may not be altered in any way.
6. Air box opening at windshield may not exceed 27 inches long and 2 1/2 inches in height. Air box cannot dissect plane of where the windshield and dash meet. Air box must be forward of this intersection.
7. Only one 4" x 14" air cleaner element or less allowed.
8. No additional fuel reservoir or pressure equalizing systems allowed. A. Fuel filter must be used on suction side of fuel pump if used.
9. Mandatory throttle return springs working in opposite directions.
10. Stock type mechanical fuel pump only, pump must mount in stock location.
11. Carburetor must be secure. No vacuum leaks, and in stock location.
12. Carburetor components may not be altered to allow any additional air, air flow, or emulsify gas with air.

**EXHAUST**

1. Maximum O.D. of plumbing 4 inches.
2. No form of spacer plates allowed except for gaskets between head and exhaust manifold.
3. Cast iron unaltered exhaust manifolds allowed.
4. Headers with a price under \$650 allowed on all engines. Tri-Y are not allowed, no Merge-Collectors. No custom, one-of-a-kind or homemade headers. No Stingers, inserts, cones, scavenge devices or any other devices allowed in header collector at any time.
5. Mufflers required (must not exceed >96dba while competing). Visiting teams are allowed an exception for two events, and are subject to a weight penalty of 25lbs.

**FUEL**

1. Pump Gasoline only. No power boosting or oxygen enhancing additives allowed. Fuel test at track is final.
2. Vacuum fuel pumps only, NO electric or pressure systems.
3. Fuel tank must be separated from driver by a complete metal firewall.
4. Fuel cell must be secured with at least a minimum of four 1 1/2" x 1/8" straps over and under the fuel cell.
5. The fuel cell itself may not have more than a 22 gallon. maximum capacity and must have an 11-inch ground clearance. (Note: Fuel cell size greater than 15 gallons must be approved by Tech Official) Absolutely no fuel cell "blocks" or any other device that will alter the fuel's location is allowed.
6. Fuel Cell Construction - 22-gauge steel with plastic or rubber bladder. Cell must have rear and front guards, baffles and foam. Cell must be protected with 1/8" steel plates on rear, right and left sides.
7. No nozzles, injectors, containers or lines for the purpose of dispersing pressurized liquids or gases - other than fire extinguishers allowed on race car.
8. Fuel cell must fit between rear clip of frame. Fuel cell must be centered (+/-1") off centerline of rear wheels.
9. Breather must exit back bumper cover on left.
10. Fuel fill to be approved by tech official. Fender fuel fill must be cabled.
11. Fuel fill cap must be secured (chained/cabled) to prevent loss. If possible, fuel shut off valve should be installed near cell.
12. Fuel will be tested by the Speedways' test equipment and must pass any measurements set forth by the Speedway.

**RADIATOR**

1. Any production radiator allowed provided it fits under the hood, in stock location. Aluminum radiators allowed with prior approval of Technical Supervisor.
2. Fan must have a shroud.
3. Overflow catch can is required. Permanently installed ahead of engine firewall.

4. Aluminum water pumps allowed.
5. No antifreeze allowed in cooling system. \$20.00 fine for any violations.

### **ELECTRICAL**

1. No magnetos, crank trigger, multiple coil or programmable systems allowed. Only one ignition system.
2. MSD/Crane ignition boxes- must have 6 pin connection. All approved ignition boxes must be mounted in plain site and out of reach of driver.
3. Dual points, breaker or breaker-less systems allowed.
4. Batteries must be securely mounted between the frame rails, forward of the rear axle and covered.
5. All cars must have an operating starter.
6. Stock 12-volt batteries only.
7. Master kill switch to the (+) positive side of battery. Switch must be located in center of car, with access from both windows.
8. Electronic traction devices not allowed.
9. Ignition wiring must be exposed.
10. ALL leads MUST be accessible and visible to Tech Staff.
11. The ignition box must be mounted to the right of the driver and easily accessed from the passenger window.
12. The ignition box must have a 6- pin female connector. The **MSD** part number **ASY-17296** wiring harness must be used. The track will have several shielded wiring harnesses that will be swapped with the competitor's harness.
13. The wiring harness path from the ignition box to distributor must be large enough for easy swapping of wiring harness.
14. No digital gauges or data loggers allowed.

### **ROLL CAGE**

1. Round steel 13/4" .095 roll-over bars are compulsory and must meet NASCAR minimums as prescribed in the Weekly Series Rule Book. Aluminum and/or other soft metals not permitted. Roll bars must be welded.
2. All cars are required to have a rear vertical hoop behind the driver's head connected to left and right front roll bar legs by a roof hoop. The front roll bar legs must follow the contour of the windshield post and cowl. The rear vertical hoop must be supported by a diagonal bar from top left to bottom right or top right to bottom left. A roll bar must connect the left and right of the rear vertical hoop at seat height. An "X" must connect left and right frame rail. An additional roll bar must be installed across the bottom of the dash board, extending from the left front roll bar leg to the right front roll bar leg. Rear support bars (no less than 1 1/2" round steel), left and right, must extend from the top of the rear vertical hoop to the rear of the frame in the trunk compartment. The front leg bars and rear vertical hoop must be connected with four horizontal door bars on the left side. The door bars on the left side, must be convex in shape and spaced from top to bottom as equal as space permits. A minimum of 4 door bars must have six (6) vertical studs equally spaced. Two angular studs must be attached from bottom door bar to main frame rail. Right side must have at least 3 door bars. Transmission hoop and front hoop are required. Butt welds, joints, and connections must have gusset plates for reinforcement.
3. Roll bars must be padded and taped from top of frame on left side to center to top.
4. Roll bar thickness will be ultrasound tested. Severe penalties will be assessed for violation of minimum thickness.
5. Top roll bar and hoop must follow contour of roof as close as possible. Roof hoop at sides or side window opening must be tight to roof as close as possible.
6. Door bar deflector plate mandatory. Door plate must be welded to roll cage. (See diagram on page 9).
7. Rear clip construction, either 'over tail' or 'under tail' conventional rear clips allowed. (Over tail is highly recommended.) Rear frame rail must be 2" x 3" box style tubing with a minimum thickness of .083.

### **MINIMUM SPECIFICATIONS**

1. At any time, before, during, or after an event, officials may require additional measures of equipment or make additional determination, as they deem necessary to further reduce the risk to competitors.
2. All cars are subject to a minimum specification inspection at any time. It is the responsibility of the driver to prepare their car FREE of defects and in safe racing condition.
3. A. A quick release-type approved safety belt of no less than three (3) inches in width is compulsory. Both ends must be fastened to roll bar cage with aircraft quality bolts not less than 3/8 of an inch in diameter. A steel plate may be welded to the roll bar cage on the right side of the drivers' so the belt can be brought down in such a manner that it will prevent the driver from sliding from side to side under the belt. The belt must come from behind the driver.
  - B. 3" shoulder harness and additional V-Type seat belt mandatory. NASCAR-approved padded headrest mandatory. Shoulder harness inertia reel cannot be used.
  - C. Additional right-side head restraints allowed.
4. Center top of steering post must be padded with at least two inches of resilient material.

5. It is required that race cars have an approved fire extinguisher with its equipment within ready reach for any emergency. Pressure-type metal containers used as part of a fire extinguisher system will only be approved for installation and discharge in the driver's compartment in conjunction with the fire extinguisher system. The container cannot be concealed in any manner and there is a limit of one container for each fire extinguisher system.
6. Driver's seat must be fastened to the frame.
  - A. Rib rest shall remain flexible and not reinforced.
  - B. Recommended seat be as far right as possible.
7. A. Window nets are required and used at all times. NASCAR type seat belt buckle method of mounting is recommended.
  - B. Window net must be full size ribbon type.
8. All drivers must be protected at all times with a flame-resistant driving suit, top and bottom. 3) Recommend undergarments be worn under driver's suit.
9. Front and rear 'bumper' ends must be capped.

#### **MISCELLANEOUS**

1. All cars must run steel floorboard under the driver, metal firewalls. No tub type interiors.
2. Side window area must remain open other than window nets.
3. No carbon fiber or titanium parts anywhere. (see engine rule)
4. Racing seat required.
5. Fire extinguisher required with a nozzle in driver's compartment and fuel cell.
6. All cars are subject to a minimum specification inspection at any time. It is the responsibility of the driver to prepare their car FREE of defects and in safe racing condition.

#### **GENERAL RULES, FLAG RULES, RACE PROCEDURES, CONDUCT AND DISCIPLINE RULES**

1. Later bulletins and/or the decisions of the race officials will take precedent and their decision will be final.
2. Protests based on NASCAR standard operating procedure.

#### **END OF LATE MODEL RULES**

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# **2022 GRAND NATIONAL SPORTSMEN**

The 2022 rules for the Sportsmen have been drafted to allow more teams to participate, and to coincide the rules package with other race tracks in the Midwest. The rules as outlined below will be applied to all cars. The Speedway may change any rule at any time in an effort to maintain equal competition, or improve safety.

**BREAK OUT RULES:** This class will have an established time of 22.000 seconds per lap for competing on the half mile. Teams are allowed to “break out” (turn a lap faster than 22.000 seconds) twice during a race event. The third time and the car will be disqualified for that event. You CANNOT “break out” on the last lap of the event. Do so, and you are disqualified. (Sportsman, Mid-American, Vintage, Trucks, 602 Late Models and Modifieds are welcome as well as any other car that meets the rules of this class). A lap faster than (TBD) may be implemented at any time. These rules are subject to change.

## **GENERAL SPECIFICATIONS**

### **BODY AND APPEARANCE**

1. No ‘ABC’ or newer release Late Model bodies allowed.
2. Cars must have full roof.
3. Stock hood must have reinforcements removed. Fiberglass hood allowed (no light weight).
4. Spoiler may not extend outside body by more than 1".
5. Bumper ends must be capped and not extend past body.
6. Stock or Lexan windshields, (1/8" minimum thickness) required, all other windows (if used) must be of Lexan. Dirt cars without windshield must have sufficient driver protection bars.
7. Body must be within 2" of outside of tires.
8. Numbers 18" high required on both sides and roof (readable from right side).
9. 1" square rub rails maximum allowed, mounted within 3" of ends. Ends must be tapered, capped, and ground smooth.

### **SEATS- BELTS- DRIVERS COMPARTMENT- INTERIOR TIMING DEVICES**

1. Approved aluminum driver's seat required. Seat must be fastened to frame/roll cage and located so that the centerline of the seat is at least 15" from driver's door bars. Properly installed headrest support required. Rib rest must remain flexible. Shoulder supports on right and left sides of seat and head support on right and left are recommended.
2. Safety belts must be dated 2014 or newer. Minimum 3" wide lap belt, 3" wide shoulder harness and submarine (crotch) strap required. Belts must be anchored to roll cage or frame. Belt anchoring's must swivel or be bolted solid pointing in same direction as when driver is belted in. Grade 5 bolts and hardware required. Shoulder harness must not be anchored lower than 2" below drivers shoulder height. 6-point belts (double crotch strap) are recommended.
3. Driver's compartment must be completely sealed off from engine compartment and fuel container and must be constructed to allow access to driver from right side by emergency personnel. Padding required around driver including steering post. Securely mounted fully charged fire extinguisher with visible dial type gauge required.
4. The window net may be a ribbon or mesh type window net with seat belt buckle release required. Window net must be permanently mounted to driver's door bars with buckle toward front roll cage upright and near roof. Clearly labeled push-pull or toggle type kill switch accessible from either side of car required.
5. Steel firewall and floor pan required on driver's side, (foot box style is OK). Passenger side interior can be tinned over from top of driveshaft tunnel to passenger side door bars.
6. No suspension components including stabilizer bar can be adjustable from the driving compartment. No carbon fiber interior components will be allowed

**CHASSIS**

1. Stock stub and fabricated frames allowed to compete.
2. In order to be considered Stock frame/chassis it must be an American made passenger car, complete stock steel frame must be retained to a point meeting or surpassing the firewall. Replacement with square tubing allowed rear of that point.
3. Fabricated chassis allowed with GM 602 Crate Motor Only.

**ROLL CAGES**

1. All cars must have a welded and gusseted 4- or 6-point roll cage made of minimum 1 3/4" x .090" wall tubing or equivalent. Roll cage must have four uprights properly welded to frame. Right side of roll cage may not be located inboard more than right rails of front and rear stubs. Main roll hoop must be behind driver and contain a diagonal bar. Cage must be reinforced from left to right with bars under the dash and behind driver in main roll hoop. At least two bars must extend from main roll cage rearward to frame kick-up or beyond. A minimum of four equally spaced horizontal bars required in driver's door, three in passenger side door. Door bars must be connected with a minimum of two equally spaced vertical studs. Two angular bars must connect bottom door bar to frame rail. All welds in cage and door structure must be gusseted. Driver's foot protection bar required. Left side door bar deflector plates of at least 1/8" steel required. Front hoop must connect to front roll cage uprights. Uni-body roll cage construction: contact competition director for guidelines.

**SUSPENSION**

1. Any stock appearing, non-progressive, magnetic steel spring with a maximum cost of \$100.
2. Steel or aluminum bodied, non-adjustable shocks with a maximum cost of \$200, KONI 30 Series shocks allowed. No bump-stop or rebound limiting suspensions allowed.

**STEERING**

1. Regionally accepted components for this level of racing will be allowed, call with questions.

**SPINDLES - HUBS**

1. Regionally accepted components for this level of racing will be allowed, call with questions.

**BRAKES**

1. Four-wheel brakes required at all times.
2. One brake bias adjuster (proportioning valve) allowed.
3. "Wheel fans" and brake blowers are allowed.
4. Floor mounted pedals allowed.

**WEIGHT**

- 1 Minimums including driver:
  - A. 2900lbs for: Sportsman OR, Mid-Am OR, Vintage.
  - B. 2700lbs for: Truck, OR 602 Late Models
  - D. 2650lbs for: Modifieds
2. Maximum 60% left side weight All weights must be properly anchored to, but not below bottom of frame rail. Ballast must be painted white and lettered with car number. No tungsten, lead shot, ball bearing type, or liquid type allowed.
- 3.No moving weight or weight moving devices allowed.

**WHEELS**

1. Aftermarket made for racing, steel wheels required, 8" maximum width.
2. Wheel studs must be long enough for threads to show on outside of lug nuts.

**FUEL AND FUEL CONTAINER**

1. Fuel cell required.
2. Fuel cell must be located behind rear axle between frame rails as far from rear bumper as possible.
3. Minimum 11-gauge (.120") container around sides and bottom of fuel cell required. Bottom of fuel cell must be at least 10" from ground. Fuel cell protection bar, 1 3/4" minimum diameter, required and must extend below fuel cell and be braced back to main frame. All vents must have valve to prevent leakage in a rollover situation. Filler tube extensions not allowed. Inlet cannot be on right side of cell. Outlet must be on top. All fuel cells and associated protection bars will be vigorously inspected.
4. Steel or made for racing fuel lines only (no aluminum), must be located outside driver's compartment.
5. Gasoline only, no oxygenated additives allowed.

**TIRES**

1. This class may utilize any treaded Hoosier Racing Tire which properly mounts on an 8" or narrower wheel.
2. Chemical treatment (softening) is not allowed. Tires must be at least 1/32 off of new tire depth measurement
3. Tires must be purchased from approved tire retailers.

**ENGINES**

1. All engines: oil pan may be no lower than bottom of cross-member. Options to correct are add to bottom of cross-member with square tubing or raise motor.
2. Crate Engines: GM 602 Crate Motor (P/N 19258602) is the only crate motor allowed and must be used as produced from factory. 602 allowed on all chassis, fabricated chassis MUST utilize 602 Crate Motor.
3. V-8 cast iron blocks and heads only. OEM type, mechanical fuel pump, in original location, required. Open Engine can only be used with stock chassis.

**CARBURETOR**

1. Limit of one carburetor. Steel or made for racing fuel lines required, metal fuel filter required.

**EXHAUST**

1. Exhaust must remain under car body.
2. Must exit behind driver, beneath car pointing downward.
3. All cars 100 decibel max, measured from middle walkway of grandstands. Effective mufflers required
4. Collector or muffler must be located rear of engine.
5. No car expelling flame, smoke or backfiring allowed

**RADIATOR AND FAN**

1. Any production radiator permitted. Under the hood in-stock location that does not require any body modifications is acceptable.
2. Radiator overflow catch can mandatory and must be mounted forward of firewall.
3. No antifreeze allowed; water only. \$20.00 penalty if found in car.

**ELECTRICAL SYSTEMS**

1. Battery must be enclosed and secured to frame and located behind driver.
2. Only 12-volt systems allowed.

**DRIVE TRAIN**

1. Stock style transmissions only.
2. Aluminum or steel driveshaft required.
3. Driveshaft must be painted a bright reflective color.
4. Safety hoop required on front half of driveshaft. Driver must be protected from driveshaft entering into driver's compartment.
5. All rear end plugs must be safety wired.

**MISCELLANEOUS**

1. No on-board timing devices allowed.
2. TRANSPONDER—Mandatory, with location of transponder 165" behind front edge of nosepiece.
3. RADIOS – Two-way communication between driver and pits not allowed. One-way scanners or receivers are required.

**LAP TIMES**

1. This class will have an established time of 22.000 seconds. (Subject to change)
2. Drivers will be allowed two (2) 'Mulligan' per race in which they can be under the set time, at that moment they will be warned but able to maintain their running position on track. If driver surpasses the 22.000 third mark again in the same race, they will be disqualified from the event and scored and payed at the rear of that event. Any driver turning a lap faster than 22.000 on the last lap will be relegated to the rear of the field and scored in that position.  
Disqualification for a lap faster than 21.400 seconds MAY BE implemented. If implemented, car must return to the pits.
3. Race line-ups will use qualifying as a basis. A dice roll, plus an inversion of up to 6, and the previous events finishing positions will determine feature line ups. Non -current season- feature winning cars may be placed higher up in the starting grid. Heat races will be based on qualifying and inverted.

**MINIMUM SPECIFICATIONS.**

It is required that race cars have an approved fire extinguisher with its equipment within ready reach for any emergency. Pressure-type metal containers used as part of a fire extinguisher system will only be approved for installation and discharge in the driver's compartment in conjunction with the fire extinguisher system. The container cannot be concealed in any manner, and there is a limit of one container for each fire extinguisher system.

Sportsmen Technical Questions – Contact Barry Folgate 608 344-0338

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***END OF SPORTSMEN RULES SECTION***

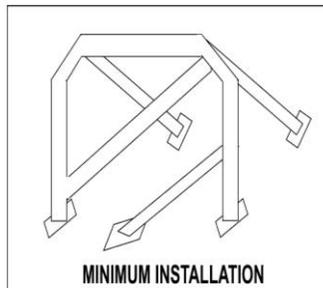
# 2022 HOBBY STOCKS

## CAR ELIGIBILITY

1. Any 1960-1996, American made car allowed. Minimum wheelbase of 108" (104" for Ford and Chrysler), no T-tops, sports cars, pony cars, or special manufactured car allowed without approval. Unitized body cars required to cross brace driver's area and have sandwich plates at roll cage mounting points.

## FRAME /ROLL CAGE

1. Frame must be unaltered from front to back. Roll cage must have 4 uprights located in the furthest forward corners of the drivers compartment and directly behind drivers' seat. Roll cage width must be symmetrical and not narrower than frame rails.
2. Roll cage must be constructed of 1 3/4" .090 wall tubing or equivalent. No pipe fittings, galvanized pipe, or exhaust tubing allowed. Roll cage must be welded to the frame or the sandwich plates if unitized body. Rear main roll cage uprights must be reinforced with 2 bars welded to the rear frame kick-up. These bars must mount to top of the frame kick-up ahead of the rear axle. Two additional bars can run from these roll cage support bars back to a point on the frame at least 16" from the rear bumper mounts. These bars may not be located within 24" of the main roll cage halo bar. Roll cage must include an "X" or diagonal brace behind the driver.
3. Three bars are required in both the drivers' and right-side doors. "Door Plates" required on driver's door. Doors maybe gutted for bar clearance. All welds must be gusseted and of good quality. Deflector plates and foot protection bar required. One bar from each roll cage upright allowed through the firewall, bar must mount to frame at or before the top of the axle kick-up and radiator protection bars. Front hoop may connect to front stub at a maximum of 6 points. Wedge (diagonal) bars allowed. Metric cars may modify firewall (moderately) for distributor clearance. Frame rails may be boxed in.
4. An "X" brace under drivers' compartment is permitted. Unit body cars allowed four bars from roll cage to rear sub-frame. Sub-frames must be connected with tubing. 6. Ground clearance- Minimum ground clearance 5 1/2' at any point (with driver), except front cross member and mufflers which is 3 1/2'.



## INTERIOR

1. Firewall and floor pan must be stock. Firewall maybe boxed to allow clearance for valve covers and distributor with engine setback. Rear firewall must be added to isolate from trunk area.

## SEAT AND DASH ASSEMBLY

1. Seat with seat belt and harness required. Racing seats allowed. Seat must be mounted in straight ahead position. If stock seats are used, a high back seat is required. Removable steering wheeled allowed, however steering shaft must remain in stock location.
2. Dash may be removed but to only accommodate a roll bar hoop.
3. Rear bottom seat cushions may be removed. Rear-seat shelf or adequate firewall is MANDATORY.

## TIRES

- 1) Only approved tire is the Hoosier 850.

**SUSPENSION**

1. Completely stock suspension components for vehicle used required. No modifications allowed except rubber bushings may be replaced with non-metallic stock replacement parts.
2. Stock ball joints (stock length and width) required. Full size spindles allowed on metric chassis. Lower control arms on metric cars may be lengthened 1". Rear stabilizer bar must be removed. Any magnetic spring over 5" in diameter allowed. Rear springs must be over 11" tall in non-metric cars. Rear springs must be over 8" tall in metric cars. No progressive springs allowed.
3. Straight valve shocks (compression and rebound must be equal) required. Shocks must be non-rebuildable and non-adjustable. Stackable shims allowed on top or bottom of coil springs.
4. Stock steering components required. No spring separators or weight jacks allowed. Stock or SRP shocks required. Max camber is 3" measured at the wheel.
5. Maximum tread width is 63", 50 lbs. penalty for each inch over max. No offset rear end bushings allowed. Stock replacement center link allowed.
6. Minimum ground clearance 6 inches. 4-inch clearance for exhaust.

**ENGINE**

1. Engine must be stock for that make. All engines may be located so that center of the furthest forward spark plug is in line with or ahead of the upper ball joint's centerline. No Chevrolet 400 CID engine blocks allowed. Maximum displacement 362 (373 for MOPAR) CID, maximum compression 9.3:1. No domed pistons. No aluminum intakes or heads. Hydraulic cam/lifters only. Lifters must pass leak-down test. Maximum lift a valve: GM .425, Ford/MOPAR .465. No matching, grinding, polishing of any type allowed. No aftermarket parts, no stud girdle, no dual point distributor. OEM type mechanical fuel pump in original location required. One OEM, unmodified two-barrel carburetor required, maximum bore diameter 1 1/4" with pressed in style booster cone. 1 3/8" on Ford, 1 9/16" on Chrysler products. No dual feed carburetors. Air cleaner and overflow tank located under hood required. The IO52 World Product stock replacement head with 171cc intake runners are the only non-OEM heads allowed. Roller tip rocker arms allowed 1.52 max. A 3/4" NPT inspection hole in oil pan required. Inspection hole must be located in line with second or third rod journal of crankshaft, on either side of pan and above sump area (oil level).
2. Crankshaft- Stock appearing crankshaft required, minimum weight #48. Crankshaft must stock appearing counter-weights and may not be scalloped or gun-drilled. Stock, unaltered flanges required. Any suspect crankshaft can be removed and weighed.
3. Intake and exhaust manifolds-Completely stock passenger car 2bbl cast iron intake and exhaust manifolds required. No grinding, polishing or altering allowed. 4bbl intake and manifolds allowed only on engines that were not offered with a 2bbl manifold. 1 1/4" adapter plates allowed on cars with 4bbl manifolds. No Bow-Tie manifolds. Internal heat riser crossover must remain in place. Center dump and log style manifolds allowed. Max exhaust manifold diameter 2 1/16". No cast iron header manifolds.
4. Aluminum pulleys allowed as long as remain stock size and configuration.

**CARBURATOR**

1. One stock Holley 4412 on one Rochester carb required. HP metering blocks not allowed.

**COOLING SYSTEM**

1. Stock OEM radiator or aluminum aftermarket radiator (\$150 max) in stock location.
2. No electric fans.
3. One-gallon overflow catch-can must be maintained inside engine compartment.
4. No extra engine coolers.
5. No antifreeze allowed.

**DRIVE TRAIN**

1. Stock automatic transmission with all forward and reverse gears operating required. Stock operating 12inch minimum diameter torque converter required. Transmission overflow bottle required. No lightweight (Vega type) torque converters allowed. No Powerglide transmissions. No rubber transmission lines allowed. Transmission cooler not allowed in driver compartment.
2. Stock, unaltered, open rear-ends required. Stock diameter steel drive shaft required. Aftermarket or S-10 right side axle required on metric cars.

**WEIGHT**

1. Minimum total weight 3400lbs. 53% front axle and 45% right side weight.
2. All ballast weights must be properly anchored to (and not lower than) frame rail outside drivers' compartment. Ballast must be painted white and lettered with car number.
3. All weights include driver.
4. Management reserves the right to adjust the weight requirements as needed to preserve competitive balance.
5. Metric weight rule- Metric cars run at 3400lbs with a right-side weight 44%.

**WHEELS AND TIRES**

1. Hoosier 850 tires required.
2. Steel wheels only. Max width 8" measured at the bead. Max offset 2". Aftermarket racing wheels required on right front. No wheel spacers allowed. No bleeders allowed. 1" lug nuts required. Heavy duty studs recommended.
3. Offset on racing type wheel 3 inches maximum measured from back side.

**GAS TANK**

1. All stock tanks must be removed. A 6 or 8 gallon marine or farm tractor tank allowed. Maximum 16 gallon fuel cell allowed.
2. Gas tank must be placed as far forward in the trunk area as possible, centered right to left. Minimum ground clearance of 10" and secured with solid metal straps, bolts and washers. No plumbers strap allowed. If trunk floor is removed, it is required to install two 2" square tube .125" wall cross members to the rear frame rails: one in front of and one behind the tank/cell.
3. Fuel line must exit top of tank. No fuel lines in drivers' compartment. Complete metal firewalls front and rear required.

**BRAKES**

1. Stock brake units only. Four-wheel brakes required at all times. Master cylinder and pedals must remain in stock location.
2. No "made for racing" components allowed. No brake motors allowed.
3. One OEM brake bias adjuster (proportioning valve) located in engine compartment allowed.

**BATTERY**

1. Batteries must be strapped and securely covered. Batteries must be safely and securely mounted. Batteries may be moved into driver's compartment, remain in stock location or ahead of rear axle and away from fuel container and lines.
2. All batteries in driving compartment must be in approved sealed battery box.
3. Maximum 12-volt system.

**EXHAUST SYSTEM**

1. Maximum diameter 2 ½" before collector and/or muffler.
2. 4 ½" maximum diameter behind muffler.
3. Exhaust must exit left or rear (not right side) and behind driver.
4. Effective mufflers required. Collector or muffler must not be located forward of transmission. 100 decibel limit. No car expelling flame, smoke or backfiring allowed.

**BODY AND TRIM**

1. Complete stock appearing steel bodies with glass or Lexan windshield (with braces) required. Full metal firewall and floorboards required.
2. Rear spoilers allowed, maximum spoiler length 3". Spoiler may not extend outside body.
3. Stock unaltered body mounts required.
4. All doors must be welded shut.
5. Fenders may be trimmed moderately for clearance. Fenders may not be inclined or bent downward more than 3" from stock. Front and rear overhangs maybe shortened maximum of 4".
6. All glass and plastic must be removed, except windshield.
7. Hood pins must replace hood and trunk latches. Hood X-brace reinforcement must be removed.
8. Limited radiator protection bars allowed. Modest use of bars on top of bumper and behind nose piece allowed.
9. Plastic nose and rear bumper covers allowed provided they are stock appearing for that make and model car.
10. Tow hooks- Tow hooks on front and rear required. ¼" cable has proved inadequate.

**BUMPERS AND BRACING**

1. Car must have complete stock bumper, do not cut off bumper ends or corners.
2. Each bumper must each have two safety chains or straps to prevent the bumper from falling off the car.
3. A "brace" or protection bar allowed on front and rear. Limited to a single bar or loop. No blunt ends and must be welded securely to frame.
4. "Bay" bars allowed as long as there are no uncovered holes in firewall.

**EXCEPTIONS**

1. Ford and Chrysler exceptions- Ford and Chrysler will be permitted some variation to improve competitiveness. Such variation requires prior approval by Director of Competition.
2. GM Metris exceptions- IMCA approved stamped steel tubular upper A-arm 8 to 8 ½" only allowed.
  - a. One inch longer than stock ball joint allowed, both sides must be the same length
3. Crate motor option- Management reserves the right to experiment with adapting a GM 602 crate motor for use in the Hobby Stock division. All carburetion, weight and other rules will be adjusted to maintain competitive balance. Contact track for additional information.

**MISCELANUOUS**

1. Safety Belts and Helmet- Belts must be dated 2015 or newer.
2. Helmet must meet Snell SA 2010 or newer.
3. Transponders required this Division.
4. No two-way radios. All drivers must have an operating scanner when on the racing surface. Scanner frequency: 454.000
5. The frequency, method, timing and type of car inspections and the number of cars to be inspected at any event will be determined by management. The management reserves the right to seal, mark or impound cars competing in their events for inspection purposes. Illegal items escaping detection during inspection does not make them legal. Illegal items will be confiscated.
7. One driver per car per night. Only one person may hot lap, qualify and race the car during the entire night.
8. A fire extinguisher must be secured and within reach of driver.
9. All cars are subject to a minimum specification inspection at any time. It is the responsibility of the driver to prepare their car FREE of defects and in safe racing condition.

**ELIGIBILITY**

Experienced drivers are acceptable but previous race driving experience may disqualify you. Our officials will resolve any questions about your eligibility. **\*\*REMEMBER!! DON'T DISAPPOINT YOURSELF IF YOU'RE A MINOR...GET YOUR MINOR RELEASE NOW! (15 years through 18 years.)**

***END OF HOBBY STOCKS RULES SECTION***

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# HORNETS

## ELIGIBLE MODELS

1. Any 3- or 4-cylinder compact car.
2. Maximum wheelbase of 103". (Wheelbase exceptions must be approved by Speedway)
3. A minimum wheelbase may be established for the **2023** season and beyond.

## INTERIOR

1. Dash, steering and pedals must remain stock. (Dash may be altered to accommodate any roll cage)
2. Folding seatback fastened in position.
3. Racing seats allowed. Quick release steering wheels allowed.
4. Interior floor pan must be complete. No holes.
5. All airbags must be removed.

## BODY

1. Sun roof openings must be closed with steel.
2. Remove all glass except windshield. Broken glass must be cleaned from inside car before arriving at speedway.
3. Driver's door must be chained or welded shut. Driver's door plate required if roll cage does not have door bars (see roll cage)
4. Passenger door must be welded or chained.
5. **Front and rear bumpers may be replaced with aftermarket products. Both front and rear bumpers may be reinforced.**
6. Hood and trunk must be secured with hood pins only. All factory hood and trunk latches may be removed. Hood may be debraced.
7. No gutting of steel body panels.
8. Working third brake light optional.
9. Minimum ride height of 4.5".

## ENGINE COMPARTMENT

1. Stock engine for that make and model from carburetor to the exhaust manifold.
2. Stock transmission.
3. All insulation under hood must be removed.
4. Battery can be relocated. If relocated, it must be placed behind the drivers' seat. All batteries must be secured by minimum 1/8" x 1" steel strap with minimum 3/8" hardware with large head washers.
5. **Exhaust must be complete and leak free.** Maximum size of 2" O.D. must run from engine to an area behind the driver. Mufflers are required to retain any sound below 96dba.

## SUSPENSION

1. Steering must remain stock with no modifications
2. Springs may be replaced but no other modifications allowed to existing stock spring buckets. Any evidence of cutting, moving or changing the geometry of the stock buckets will result in not passing tech. Tabs or other retainers are allowed in stock buckets to hold springs in place.
3. 1" camber allowed. See Tech Man for system used to measure camber.
4. Maximum 1" toe in or toe out allowed.
5. Minimum ride height of 5 1/2". A tolerance will be applied if the OEM ride height is lower. Exhaust not included in measurements.
6. A strut bar is allowed.

## COOLING SYSTEM

Any radiator allowed which will fit in stock mounts. An overflow container mounted in engine compartment. Water only in radiator. No anti-freeze.

## GAS TANK

If gas tank is ahead of rear axle it may remain in place. If gas tank is behind axle it must be placed with a fuel cell or marine tank (max. 6.6 gal.). Must be secured by minimum 1/8" x 1" steel strap with minimum 3/8" hardware. Must be covered in hatchback in a metal case.

**TIRES / WHEELS**

1. Tires may not have a retail value (to all) greater than \$80.
2. No racing tires. No autocross, no SCCA approved tires, no competition DOT. All tires subject to Speedway approval.
3. Maximum footprint of 7". Tires must have same size and profile on all 4 corners
- 4.. Wheels must be of same size. Aftermarket racing wheels allowed. Wheels stock off-set or not to exceed 4".

**WEIGHT**

1. Weight distribution must remain as stock.
2. A minimum weight of 2200 lbs. for single overhead cam cars.
3. A minimum weight of 2400 lbs. for dual overhead cam cars.
- 4) Any ballast added must be within 18 inches of centerline of car. Note: Door plates are not considered ballast and can be added to both sides of car.
- 5) Weight reductions or additions may be implemented based on the engine's horsepower as determined by manufacture.

**SAFETY**

1. Helmet must meet minimum DOT standard, suggested Snell approved. Gloves and long sleeves required.
2. Fire extinguisher required. 2 lb. BC rated mounted in quick release bracket within reach of driver.
3. Window nets **required**. Racing seat belts are recommended.

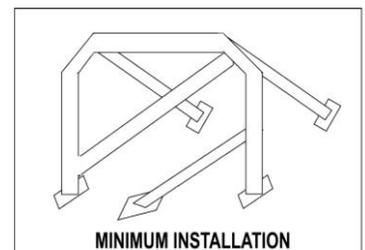
**ROLL BAR AND SIDE BARS**

1. All vehicles must be equipped with a roll bar. The material used must be 13/4 x .095 or better milled steel. No pipe fittings, exhaust pipe, or conduit.
2. Door bar or plate **REQUIRED**.
3. The roll bar must be located directly behind the driver and reach as close as possible to the roof and the left side of the car.
4. Two support bars must be added to roll bar to form a triangular, strong structure.
5. Driver compartment bracing must remain inside the driver's compartment and may not dissect, bisect or pass through either the engine or trunk firewall. Such bracing may be installed as a safety feature only and may not reinforce the body or frame or alter the geometry of the car.
6. Roll bar installation is subject to inspection which will be closely scrutinized. Refer to illustration for possible designs.

NOTE: Four door hardtops (no door post must have door bars or plate as described below.  
A flush, securely mounted exterior driver's door protection plate is permitted as an option.

NOTE: Plate must be same color as car body (entire door area may be white) and properly numbered.

NOTE: The plate should over-lap the front and rear door edges by at least 12 inches.

**CAR NUMBERS**

1. Minimum 18" high numbers in contrasting color on both doors and roof.
2. 6" number on upper right corner of windshield and on back of trunk - right side above tail light.
3. NO spray painted or "duct" tape numbers. Cars with non-legible numbers will not be scored.

**GENERAL**

All cars are subject to a minimum specification inspection at any time. It is the responsibility of the driver to prepare their car **FREE** of defects and in safe racing condition.

**MISCELLANOUS**

1. Scanners required. Frequency is 454.000. Transponders required. Location of unit determined by track.

**END OF HORNET RULES SECTION**

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# **STREET STOCKS / MINI-VANS**

## **ELIGIBLE MODELS**

1. Any 6-cylinder stock OEM car, truck or van. Minimum wheelbase of 103"
  - a) No engines greater than 3.8 liters for Street Stocks
  - b) Mini Vans must have a sliding side door to compete in Mini Van Division.
2. No four-wheel drive (activated) vehicles allowed. Rear wheel drive cars allowed.
3. No manual transmissions allowed.

## **INTERIOR**

1. Dash, steering and pedals must remain stock. (Dash may be altered to accommodate any roll cage)
2. Folding seatback fastened in position.
3. Racing seats NOT allowed.
4. Interior floor pan must be complete. No holes.
5. All airbags must be removed.

## **BODY**

1. Sun roof openings must be closed with steel.
2. Remove all glass except windshield. Broken glass must be cleaned from inside car before arriving at speedway.
3. Driver's door must be chained or welded shut. Driver's door plate recommended.
4. Passenger door chained only.
5. Bumpers must be chained to frame.
6. Hood and trunk may be secured with hood pins.
7. No gutting of steel body panels.
8. Working third brake light optional.
9. Stock ride height must be maintained.

## **ENGINE COMPARTMENT**

1. Stock engine for that make and model from carburetor to the exhaust manifold.
2. Stock transmission.
3. Insulation under hood may be removed.
4. Battery must be in stock location. Secured by minimum 1/8" x 1" steel strap with minimum 3/8" hardware with large head washers.
5. Exhaust complete and leak free. Maximum size of 2" O.D. must run from engine to an area behind the driver. Mufflers are required.

## **SUSPENSION**

1. Steering and suspension must remain stock. No modifications. Ride attitude must be maintained.
2. 1" camber allowed. Measured with a carpenter square.
3. No ballast may be added.
4. Maximum 1" toe in or toe out allowed.

## **COOLING SYSTEM**

Any radiator allowed which will fit in stock mounts. An overflow container mounted in engine compartment. Water only in radiator. No anti-freeze.

## **GAS TANK**

If gas tank is ahead of rear axle it may remain in place. If gas tank is behind axle, it must be placed with a fuel cell or marine tank (max. 6.6 gal.). Must be secured by minimum 1/8" x 1" steel strap with minimum 3/8" hardware. Must be covered in hatchback in a metal case.

**TIRES**

1. Tire size must remain stock OEM size.
2. Must be DOT approved.
3. No racing tires.
4. Only tires speed rated M,N,P,Q,R,S,T or H allowed.
5. No tires rated U,V(VR), W(ZR), Y(ZR) or Z allowed.
6. Maximum footprint of 6.5"
7. Same size and profile on all 4 corners
8. Wheel size must be for that make and model. Same on all corners.

**SAFETY**

1. Helmet must meet minimum DOT standard, suggested Snell approved. Gloves and long sleeves required.
2. Fire extinguisher required. 2 lb. BC rated mounted in quick release bracket within reach of driver.
3. Window nets and racing seatbelts are recommended.

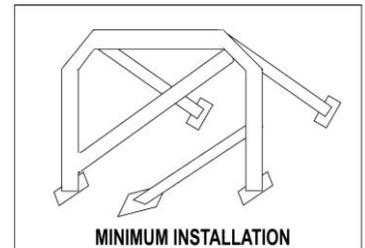
**ROLL BAR AND SIDE BARS**

1. Recommend vehicles be equipped with a roll bar. The material used must be 13/4 x .095 or better milled steel. No pipe fittings, exhaust pipe, or conduit.
2. Door bar or plate REQUIRED for four door hard tops with no door post.
3. The roll bar must be located directly behind the driver and reach as close as possible to the roof and the left side of car.
4. Two support bars must be added to roll bar to form a triangular, strong structure.
5. Driver compartment bracing must remain inside the driver's compartment and may not dissect, bisect or pass through either the engine or trunk firewall. Such bracing may be installed as a safety feature only and may not reinforce the body or frame or alter the geometry of the car.
6. Roll bar installation is subject to inspection which will be closely scrutinized. Refer to illustration for possible designs.

NOTE: Four door hardtops (no door post must have door bars or plate as described below.  
A flush, securely mounted exterior driver's door protection plate is permitted as an option.

NOTE: Plate must be same color as car body (entire door area may be white) and properly numbered.

NOTE: The plate should over-lap the front and rear door edges by at least 12 inches.

**CAR NUMBERS OR "NAMED CARS"**

1. Minimum 18" high numbers in contrasting color. Without a "named car" Numbers must be on both doors and roof.
2. 6" number on upper right corner of windshield and on back of trunk - right side above tail light.
3. NO spray painted or "duct" tape numbers. Cars with non-legible numbers will not be scored.

**GENERAL**

All cars are subject to a minimum specification inspection at any time. It is the responsibility of the driver to prepare their car FREE of defects and in safe racing condition.

**CLAIM RULE**

1. Claim Rule: Management reserves the right to claim any car at any time for \$500 which will include a complete running car minus the general safety devices. A refusal in claim will result in disqualification for event and suspension of driver and car for remainder of season.

**ELIGIBILITY**

1. Street Stock Champion ineligible to compete the following season in Street Stocks.

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# 6 SHOOTERS

## **MODELS:**

1. 2006 and older, V-6 powered, General Motors, Ford Motor Company or Mopar production front wheel drive passenger cars with a factory minimum wheelbase of 103". Convertibles not allowed. (Some examples would be: GM: Century, LeSabre, Cutlass, Grand Am, Grand Prix, Ford: Taurus, Sable, etc. Mopar: Intrepid, Concord) Foreign models are NOT allowed. No modifications or alterations except as specifically allowed in these rules.

## **BODY:**

1. Complete full body cars only with no trimming or other modifications to the body. No gutting of doors, hood or trunk allowed. All interior upholstery, except for driver's seat and driver side door panel or padding, must be removed. The dash and instrument cluster must remain intact. All airbags must be deactivated. All exterior lights, brake lights, lenses and reflectors must be removed. Third brake light must be operable. Light holes may be covered with sheet steel to improve the cars appearance. Front and rear firewalls and floor must be complete, and all holes covered with sheet steel. Sun roof openings must be closed off with sheet steel. Door plate highly recommended at a minimum 10" high, 1/4" thick steel and extend a minimum of 4" past front and rear edges of driver's front door. Door plates if utilized, must be securely mounted to the door and/or fender only, by welding or bolting or both.

## **BUMPERS:**

1. Stock bumpers for car only, and must be secured with chains or welded to prevent them from falling off. Exposed bumper ends must not have sharp edges.

## **GLASS:**

1. All side windows and rear window must be removed. Broken glass must be cleaned from inside the car before arriving at the speedway. The original windshield must be used, severely cracked may require stock replacement. One stock inside mirror only; all outside mirrors must be removed.

## **ROLL CAGE:**

1. A two-point roll cage is recommended. Must be made of minimum 1 3/4" OD mild steel tubing with a minimum wall thickness of .090". No weld elbows or fittings. Only mandrel bends are allowed. Cage design is a main hoop with a diagonal brace, positioned behind the driver's seat, extending from the floor to the roof, and extending the width of the driver's compartment. This main hoop should be braced from the top of the hoop, down and rearward to the lower rear section of what was the back seat area; two braces, one on each side. No bars are allowed to pass through the front or rear firewall. No bars may extend beyond the contour of the body.

## **ENGINE:**

1. V-6 engines only with a maximum displacement of 3.8 liters. The engine must be original or an exact replacement as offered in make and model of your car. The engine must remain completely stock. No turbo-chargers or super-chargers.

## **ELECTRICAL:**

1. 12-volt electrical system only. Battery must remain in stock location. Battery must be secured with a minimum of 1/8" thick x 1" wide steel strap, and 5/16" bolts. OEM stock sensors and computers only. OEM stock computer chips only. No tuner, performance or programmable chips allowed. OEM stock wiring harness must be used. No re-wiring without official's approval.

## **COOLING SYSTEM:**

1. Stock radiator, or any other radiator that fits in the stock mounts. The A/C condenser and compressor may be removed, but all other A/C components must remain. 2 electric cooling fans may be used. A coolant recovery bottle is mandatory. Water only in the cooling system. NO ANTIFREEZE.

## **EXHAUST:**

1. 2 1/4" O.D. maximum. Must exit behind the driver and under car.

**FUEL SYSTEM:**

- 1) Fuel pump must have no power when car is off. No fuel lines allowed in driver's compartment.
- 2) Fuel Tank: The stock fuel tank may be used if it is located forward of the rear axle; stock fuel fill must remain. A fuel tank (fuel cell or 6.6-gallon marine tank) may be mounted in the trunk, secured with minimum of 1/8" x 1" steel straps and 3/8" bolts.

**DRIVE LINE:**

- 1) Completely stock driveline only.

**SUSPENSION:**

- 1) The suspension system must remain completely stock. The springs may not be heated or cut. OEM stock for your make and model, springs, control arms, bushings, shocks, struts, spindles, etc. only. Purposeful camber not allowed.

**STEERING:**

- 1) The steering system must remain completely stock.

**BRAKES:** Stock brakes for car only. Rotor and/or caliper size may not be changed. Stock master cylinder only. Anti-lock braking system may remain functional. Brake bias adjusting devices are not allowed.

**TIRES/ WHEELS**

- 1) All 4 tires must be the same size and aspect ratio, examples: 215/70R15, or 225/60R16. Steel belted DOT radial tires only, 60, 65, 70, or 75 series. No made for racing tires.
- 2) Wheels: Stock wheels only; steel wheels are recommended. All 4 must be the same size; 14", 15" or 16".

**WEIGHT & GROUND CLEARANCE:**

- 1) No ballast allowed. Stock ground clearance must be maintained. Cars appearing too low will be subject to disqualification.

**SEAT:**

- 1) The original seat must be used. The seat back must be locked into position. Racing seats not allowed.

**SAFETY:**

- 1) A driver's window net is recommended, with release at top front of net. The center of the steering wheel must be padded.
- 2) Racing seat belts are recommended.
- 3) A fully charged dial type fire extinguisher, minimum 2 lb. BC, securely mounted, within the driver's reach, is required.
- 4) Helmet required, minimum DOT standard, Snell SA recommended
- 5) Eye protection and racing suit recommended. Long sleeve shirts, long pants, closed toed shoes and gloves required, No Nylon or similar material allowed.

**CLAIM RULE:**

- 1) Management reserves the right to claim any car at any time for \$450 which will include a complete running car minus the general safety devices. A refusal in claim will result in disqualification for event and suspension of driver and car for remainder of season.

**MISCELANOUS:**

- 1) This class is designed as a true entry level division. Drivers registered in other MIS divisions will not be allowed to compete.
- 2) Track Champion ineligible to compete the following season in this Division.
- 3) No two-way radio communication allowed. This class will NOT utilize transponder scoring. A team may use a transponder for their own information.
- 4) Race lines will be based on season points or qualifying.
- 5) Have fun.

**END OF 6 SHOOTER RULES**

**FOR HSRA**  
**(HIGH SCHOOL RACING ASSOCIATION)**  
**RULES AN REGISTRATION**  
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**[WWW.HIGHSCHOOLRACING.ORG](http://WWW.HIGHSCHOOLRACING.ORG)**

# LATE MODEL/SPORTSMEN TIRE RULES

## LATE MODELS

- 1) A team may purchase only one tire per night, per car. Tire must be run in (at least) the feature.
    - A) Each tire will be coded to assure that each NEW tire is used in feature.
    - B) A tire will be considered new until it completes at least one half of any feature.
  - 2) Tires will be plated "Lax", or may have a radio chip implanted in it, and/or have a unique symbol.
    - A) Every car registered at La Crosse Fairgrounds Speedway must have four properly plated tires by the second night of competition.
    - B) Non-registered cars must have properly plated tires in the following order:  
 First Night – One. Second Night – Two. Third Night – Three. Fourth Night - ALL  
 (All non-registered cars will start no greater than eighth position for the first two FEATURES)
    - C) Teams may purchase tires from other teams; however, purchased tires will be treated as a "new" tire.
  - 3) Any team with four new tires must start at rear of field. Any team that is allowed to purchase tires after the season has started is subject to the following parities: Four tires - must start at rear of all fields for two weeks, behind inversion third week, eligible for new purchase after second week. Three tires - must start at rear of all fields for two weeks, eligible for new purchase after second week. Two tires - must start at rear of all fields for one week, behind inversion second week, eligible for new purchase after second week. All teams must have at least two used tires for any extended lap features. Any non-registered team must have at least two USED properly plated and approved tires to start in any feature after July 1<sup>st</sup>.
  - 4) NEW cut or damaged tires may be replaced if tire has code for that night, providing tire has not completed one half of the feature laps. Damaged tire must be inspected by Tire Marshal to approve new tire for following week.
  - 5) Teams may purchase six new tires on first night of competition. Teams may purchase one new tire on the third night of competition. One per night thereafter. On double features nights, two tires MAY be purchased. Tires must be run in at least one of the feature events. New tire use is subject to tech approval.
  - 6) Tires follow the driver, not the car. (Note: Some exceptions may apply, but only with the approval of the Tire Marshal.)
  - 7) Penalties - Penalties subject to discretion of officials.
  - 8) Only registered tires from the previous season may be registered (and coded if needed) with the Tire Marshal prior to the beginning of the season. 6 for Late Model, 8 for Sportsmen.
  - 9) Cars participating in the semi feature are eligible for a new tire the following week, if they completed all laps in race.
  - 10) **Speedway Officials and management may make amendments or changes to these rules at any time.**
  - 11) All teams must submit a tire usage card before qualifying. Any team not submitting their usage card will be ineligible for a new tire at their next available race night, and may have to start at rear of field.
  - 12) Teams must qualify on the tires they will use in the feature event. On race nights, where two tires are allowed to be purchased by all teams, only ONE new tire may be used for qualifying. Any changes other changes or situations must be approved by Tire Marshal.
- NOTE: TIRES ARE AN IMPORTANT COMPONENT IN KEEPING THE RACING COMPETITION FAIR AND EQUAL. ANY ATTEMPT TO CIRCUMVATE THESE RULES COMES WITH A HEAVY PENALTY. DON'T DO IT.

## SPORTSMEN

See Tire Rules in Sportsmen rule section.

**END OF 2022 LA CROSSE SPEEDWAY RULEBOOK**